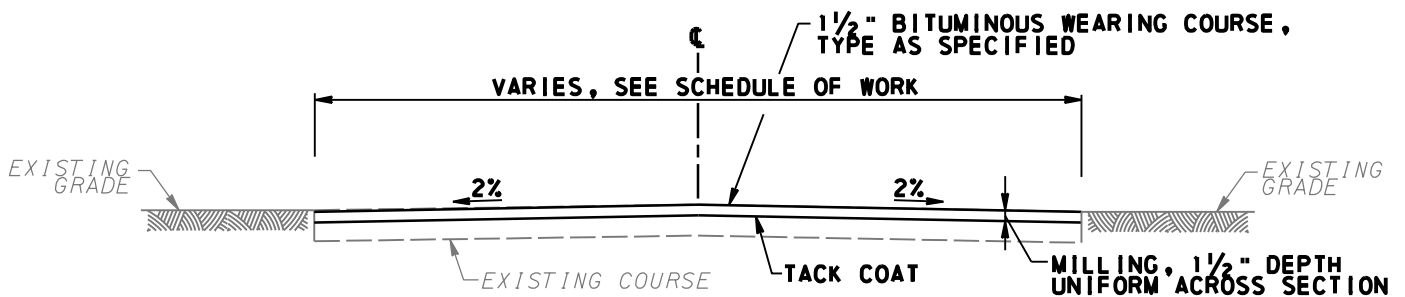


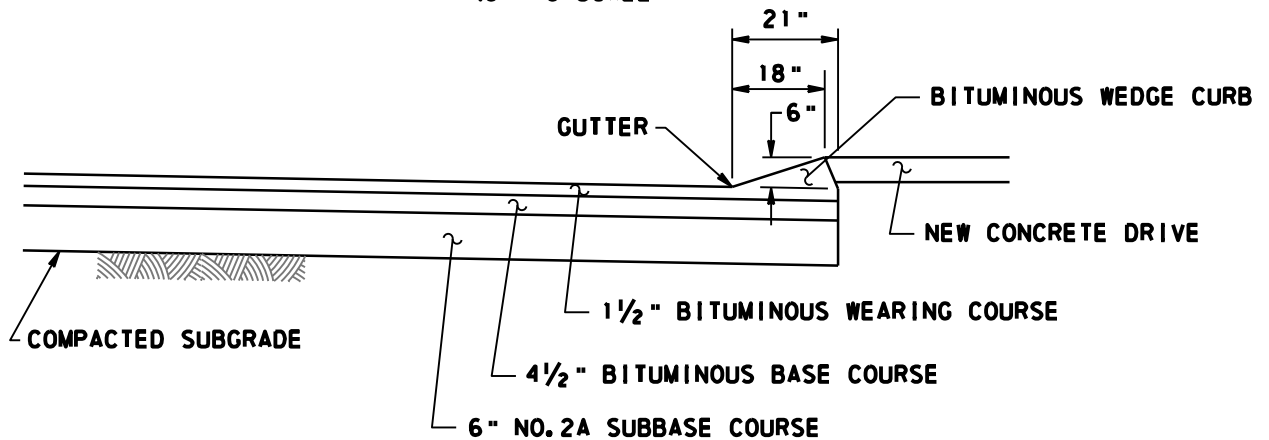
**PAVEMENT REPAIR:
TYPICAL SECTION (WITH CURBS)**

NOT TO SCALE



**PAVEMENT REPAIR:
TYPICAL SECTION (WITHOUT CURBS)**

NOT TO SCALE



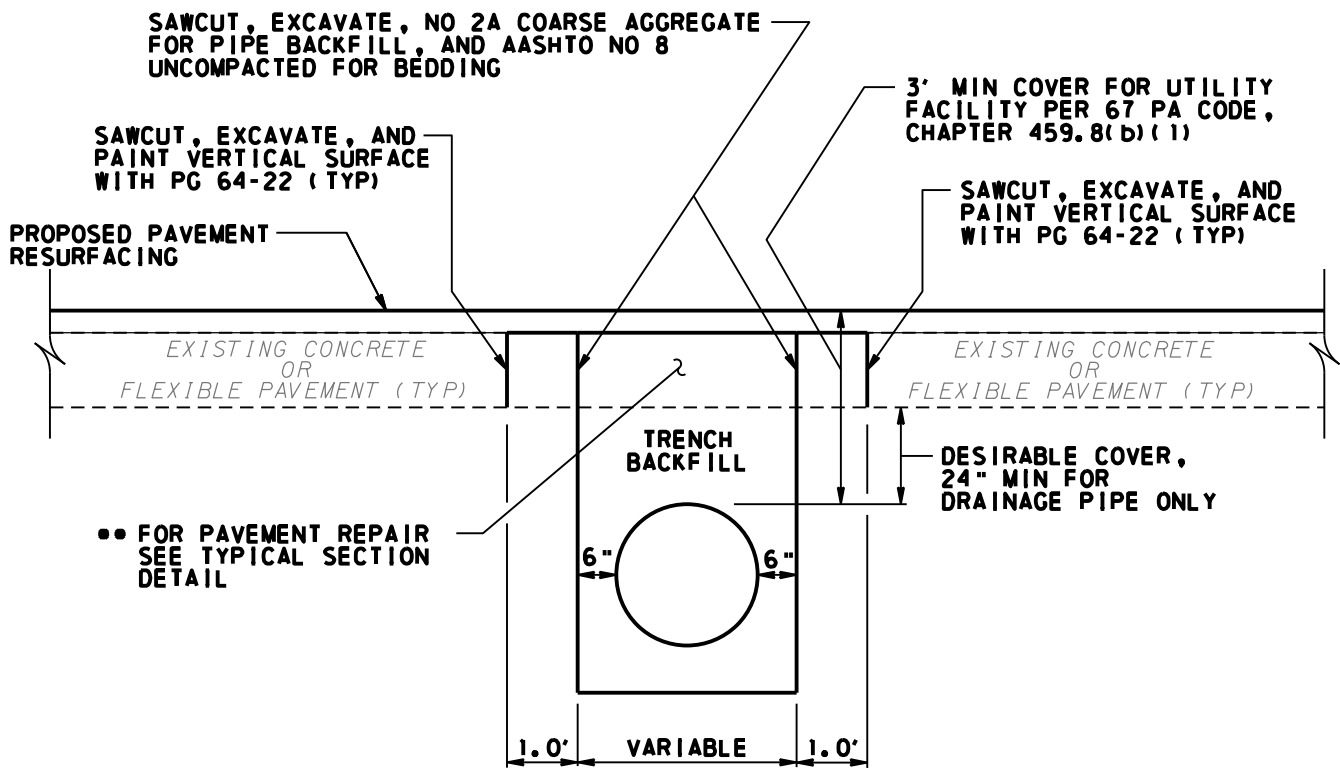
**ENLARGED DETAIL OF FLEXIBLE PAVEMENT
FULL REPLACEMENT**

NOT TO SCALE

NEW BRIGHTON BOROUGH

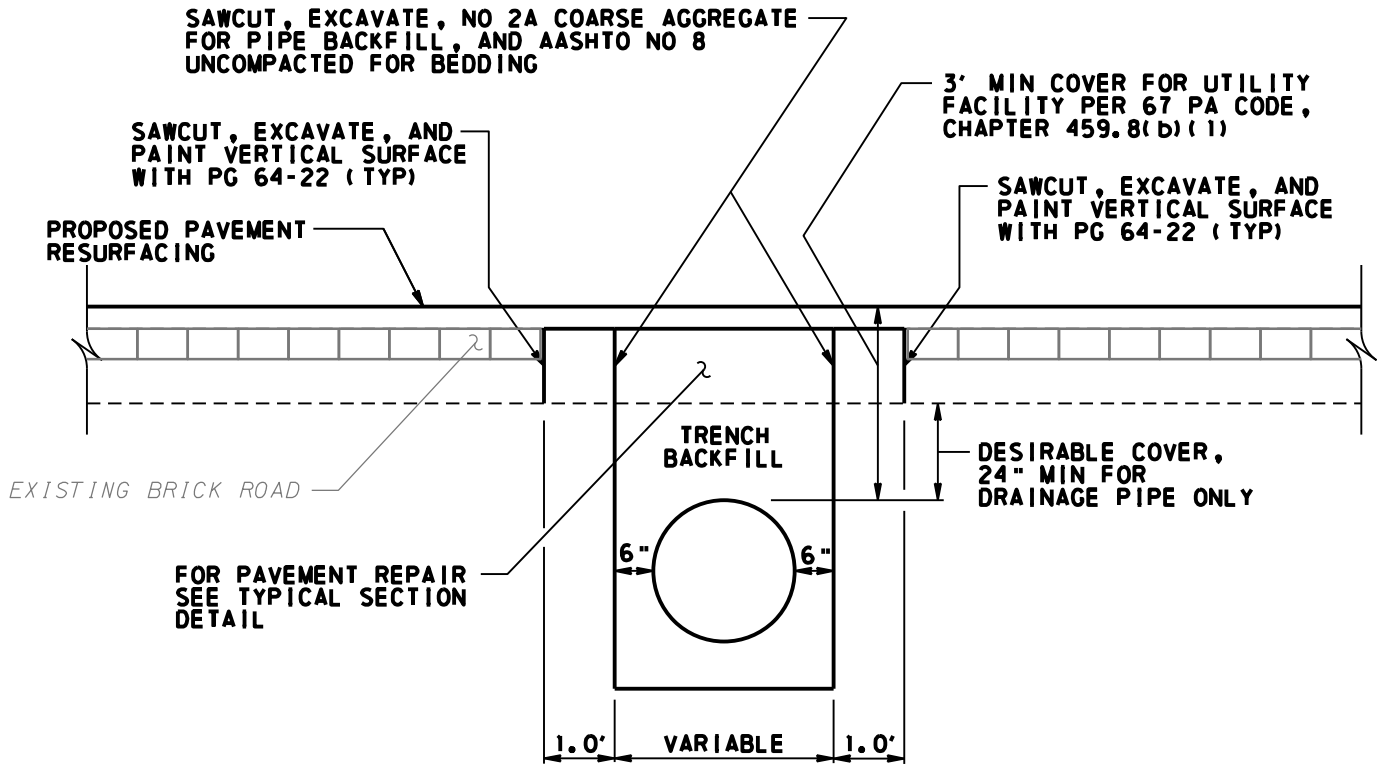
PAVEMENT REPAIR
TYPICAL SECTION

WIDMER ENGINEERING, INC.



•• IF EXISTING CONCRETE PAVEMENT IS THICKER THAN 6", INCREASE THE BITUMINOUS CONCRETE MATERIAL TO MATCH THE EXISTING CONCRETE DEPTH

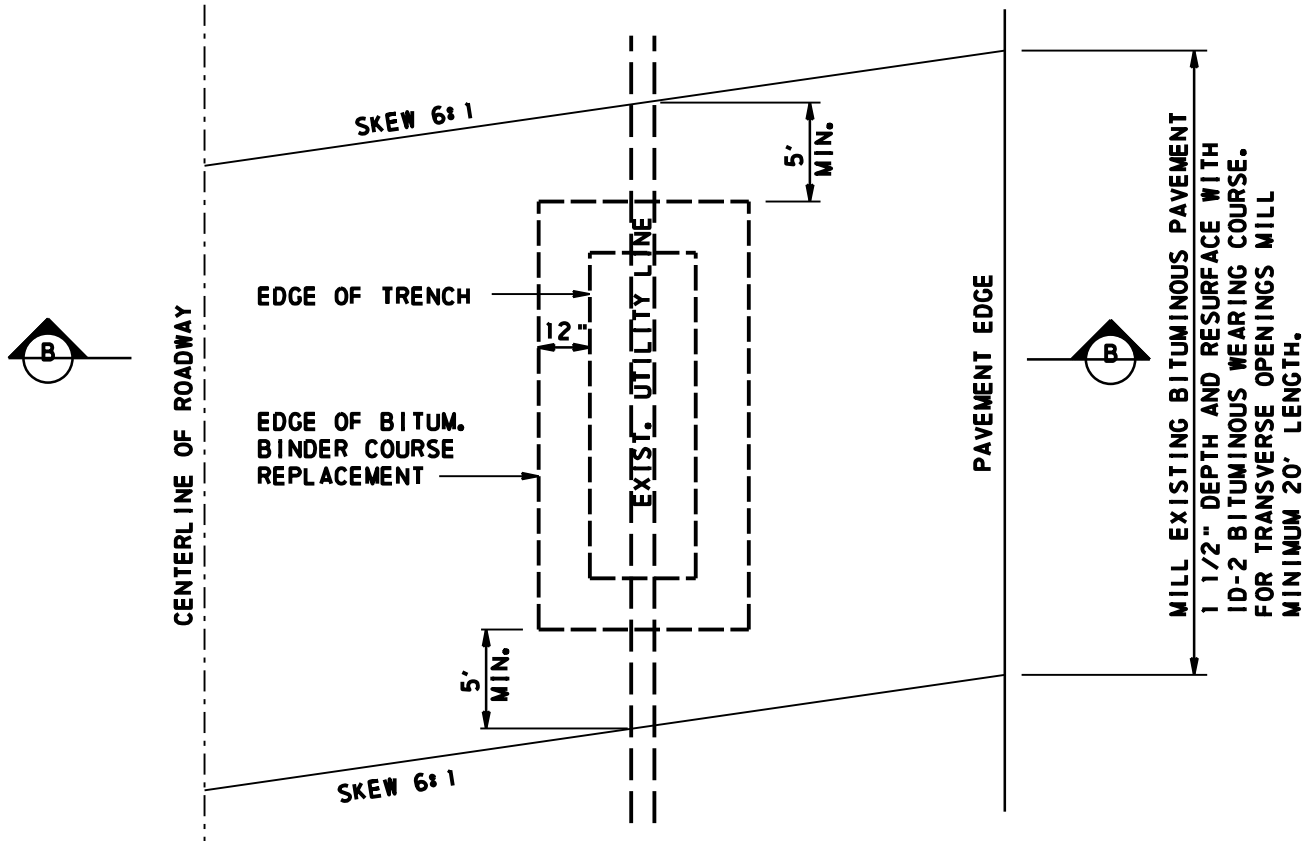
NEW BRIGHTON BOROUGH
UTILITY TRENCH: PAVEMENT RESTORATION
WIDMER ENGINEERING, INC.



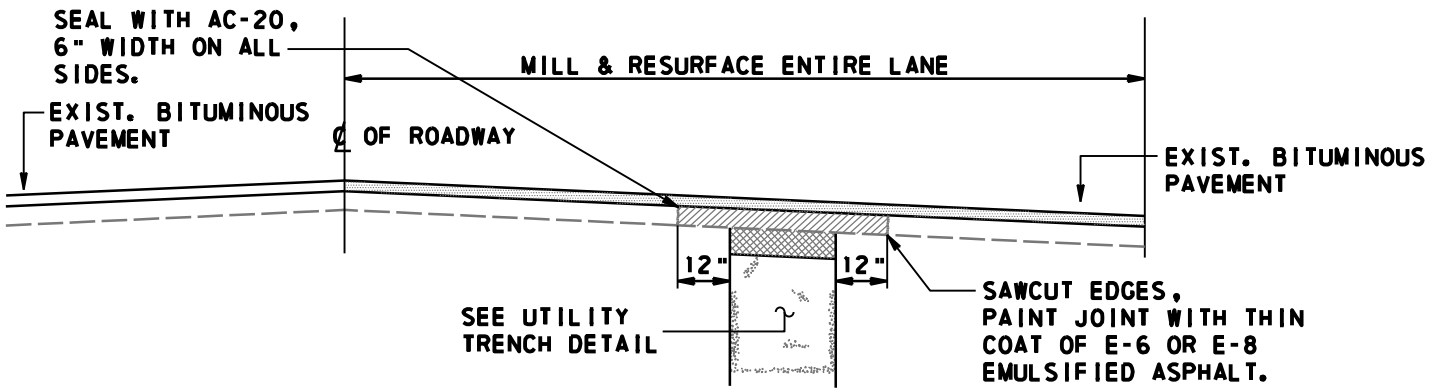
NOTE:
 PLACE TACK COAT THEN PAVE ENTIRE
 ROAD FULL WIDTH WITH 2" WEARING COARSE

NEW BRIGHTON BOROUGH
BRICK ROAD UTILITY TRENCH: PAVEMENT RESTORATION
WIDMER ENGINEERING, INC.




(APPLICABLE ONLY TO ROADS RESURFACED IN THE LAST FIVE (5) YEARS.)



PLAN - PAVEMENT RESTORATION DETAIL AB'



SECTION B-B

-  1 1/2" BITUMINOUS WEARING COURSE,
-  4.5" BITUMINOUS CONCRETE BASE COURSE,
-  6" No. 2A SUBBASE

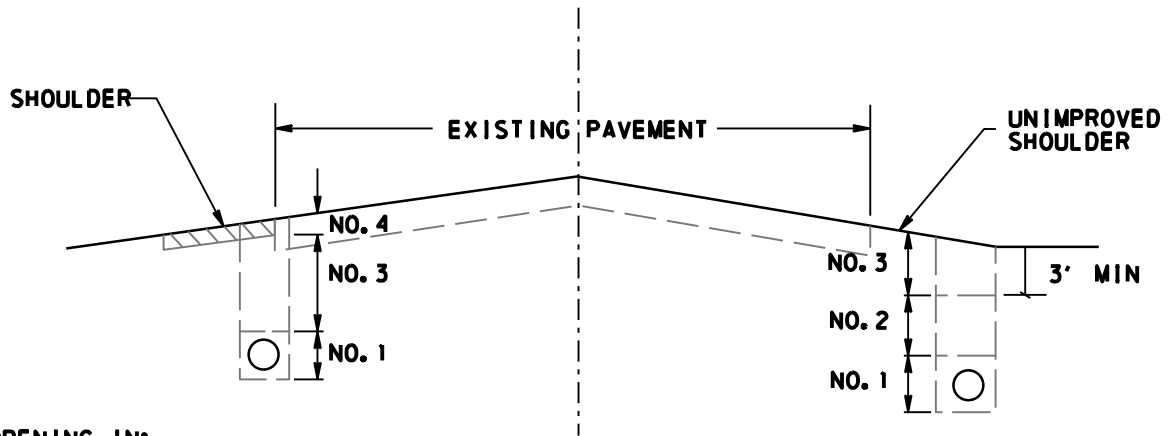
NOTE:

1. 2" MINIMUM BITUMINOUS COLD PATCH (TEMPORARY SURFACE) REQUIRED IN TRENCH AREA PRIOR TO FINAL SURFACING.
2. IF UTILITY OPENING EXTENDS INTO OR INVOLVES BOTH LANES THE ENTIRE CARTWAY SHALL BE MILLED AND RESURFACED.
3. THE MINIMUM LENGTH OF THE MILLED AND RESURFACED AREA SHALL BE 20 FEET.

NEW BRIGHTON BOROUGH

STANDARD BITUMINOUS PAVEMENT REPAIR

WIDMER ENGINEERING, INC.



OPENING IN:
 -PAVEMENT, OR
 -PAVED SHOULDER, OR
 -IMPROVED SHOULDER, OR
 -UNIMPROVED SHOULDER
 WITHIN 3 FEET OF
 PAVEMENT EDGE

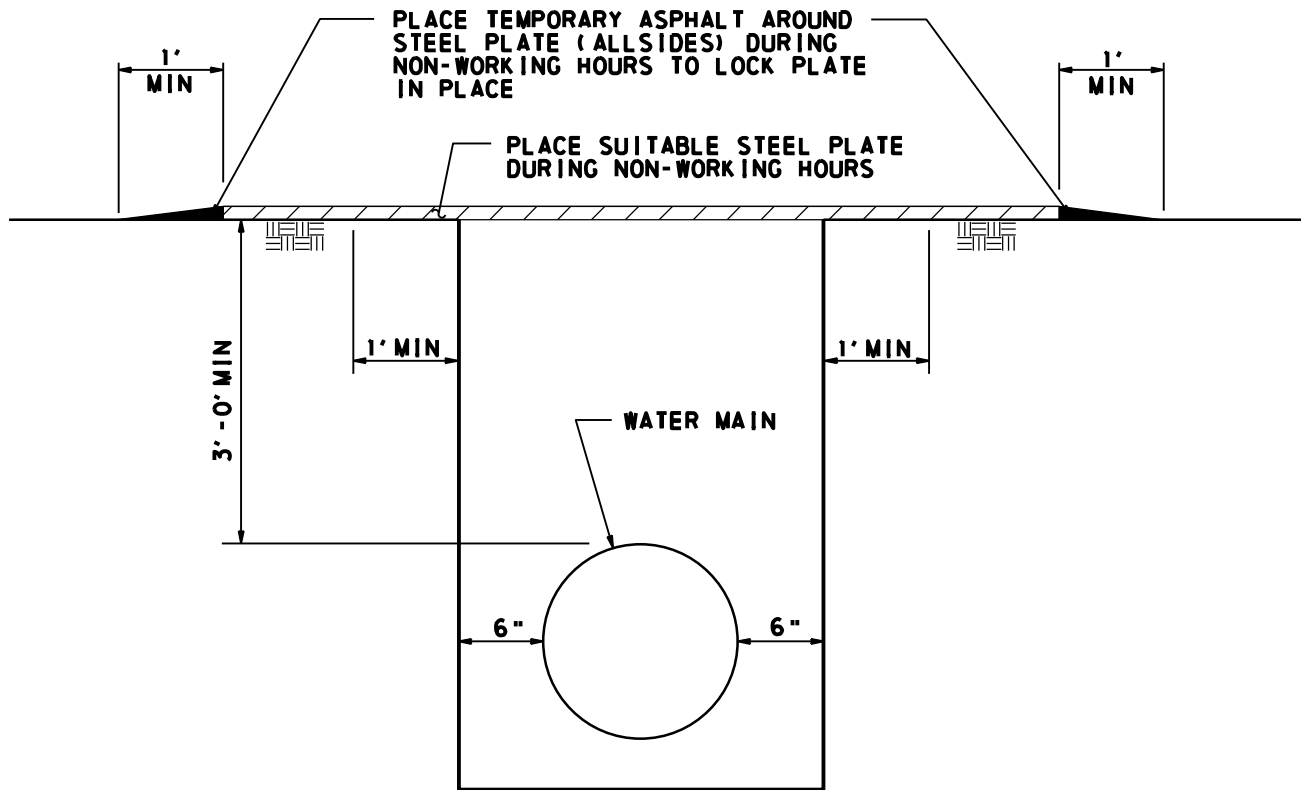
NOT TO SCALE

**OPENING IN
 UNIMPROVED
 SHOULDER MORE
 THAN 3' OUTSIDE
 PAVEMENT EDGE**

- NO. 1 PLACE FINE AGGREGATE OR GRANULAR MATERIAL UP TO 1 FOOT OVER TOP OF THE FACILITY, AND COMPACT IN 4" LOOSE LIFTS.
- NO. 2 PERMIT MAY AUTHORIZE RETAINED SUITABLE MATERIAL, IN LIEU OF #3 BELOW, AND COMPACT IN ACCORDANCE WITH DEPT. REGULATIONS 459.8(g).
- NO. 3 PLACE SELECTED GRANULAR MATERIAL AND COMPACT IN ACCORDANCE WITH PENNDOT STANDARDS TO 100% OF THE DETERMINED DRY WEIGHT DENSITY.
- NO. 4 PER PAVEMENT REPAIR TYPICAL SECTION

NOTE: FOR OPENINGS BEHIND CURB OR OUTSIDE SHOULDER, SUITABLE MATERIAL WILL NORMALLY BE AUTHORIZED FULL DEPTH

NEW BRIGHTON BOROUGH
TYPICAL BACKFILL SECTION
WIDMER ENGINEERING, INC.

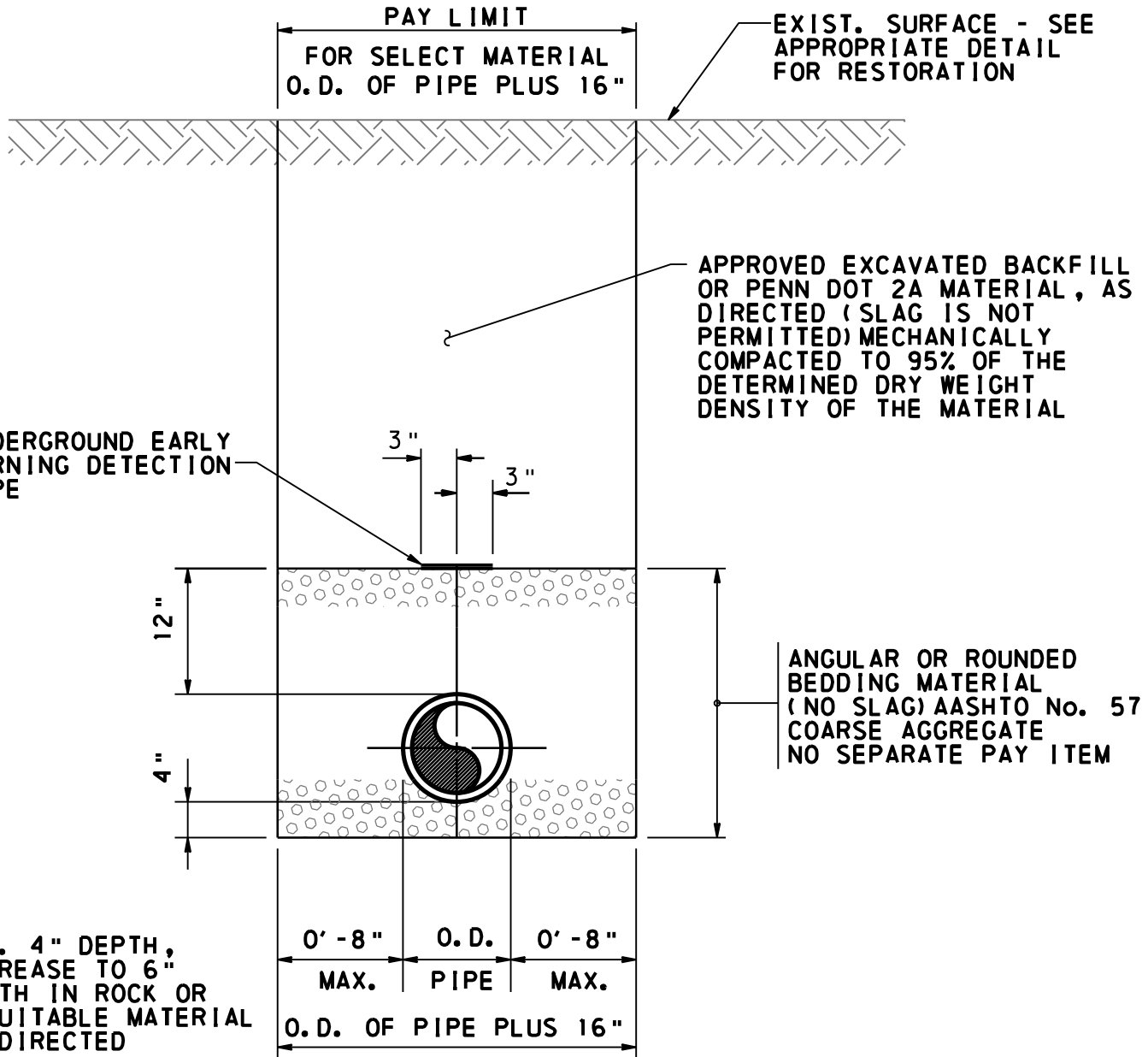


WATERLINE TRENCH DETAIL
NON-WORKING HOURS

NEW BRIGHTON BOROUGH

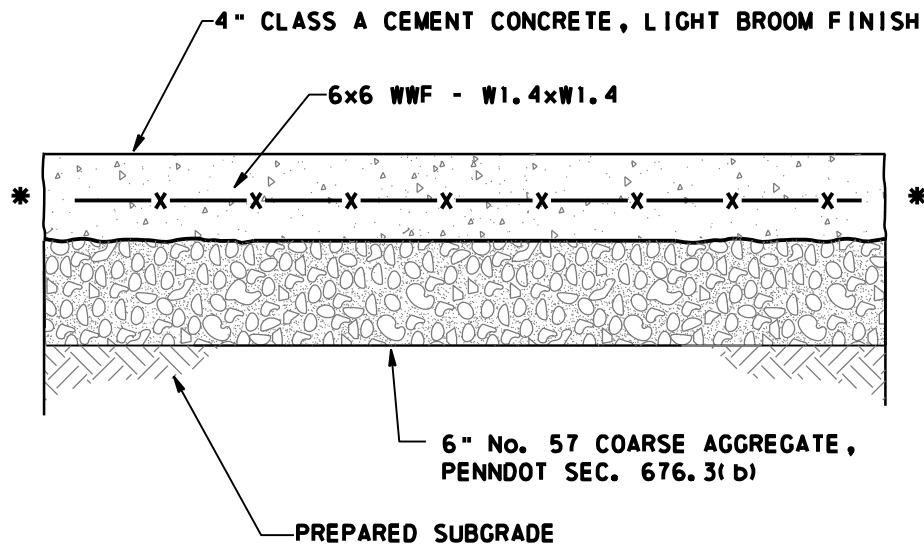
WATERLINE
TRENCH DETAIL
NON-WORKING HOURS

WIDMER ENGINEERING, INC.



MIN. 4" DEPTH,
 INCREASE TO 6"
 DEPTH IN ROCK OR
 UNSUITABLE MATERIAL
 AS DIRECTED

NEW BRIGHTON BOROUGH
STANDARD TRENCH EXCAVATION DETAIL GRAVITY SANITARY SEWER
WIDMER ENGINEERING, INC.



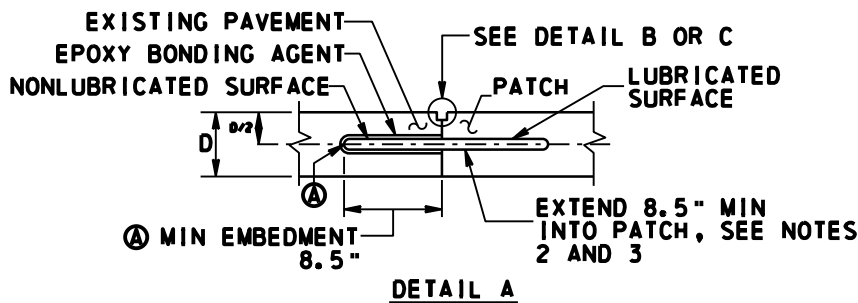
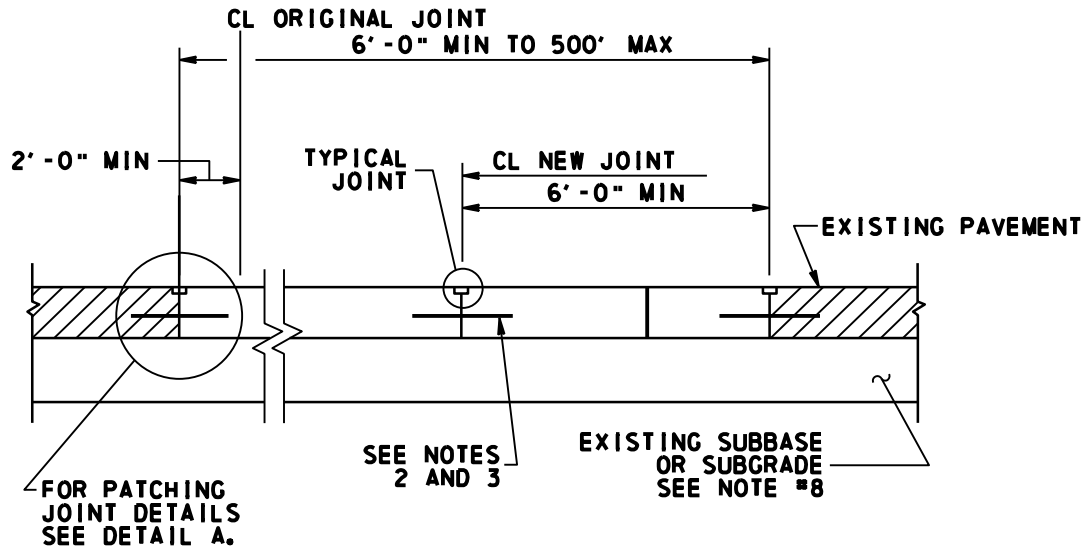
NOTES:

1. FORM OUTSIDE EDGES AND JOINTS WITH 1/4" RADIUS EDGING TOOL.
2. FORM TRANSVERSE DUMMY JOINTS @ 5 FOOT INTERVALS, APPROX. 1/8" WIDE AND AT LEAST 1 INCH DEEP.
- 3. WHEN BUTTING INTO EXISTING BITUMINOUS, SAWCUT BITUMINOUS AND PAINT BITUMINOUS WITH A THIN COAT OF E-6 OR E-8 EMULSIFIED ASPHALT.
- 4. WHEN BUTTING INTO EXISTING CONCRETE, SAWCUT CONCRETE AND USE 1/2" PREMOLDED EXPANSION FILLER FULL DEPTH OF SIDEWALK.

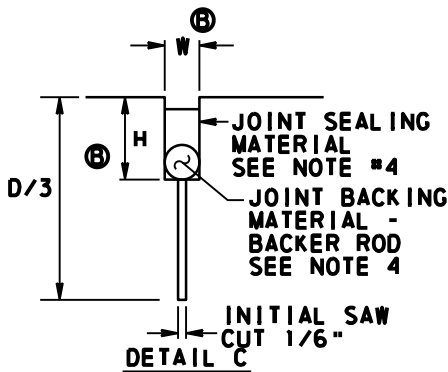
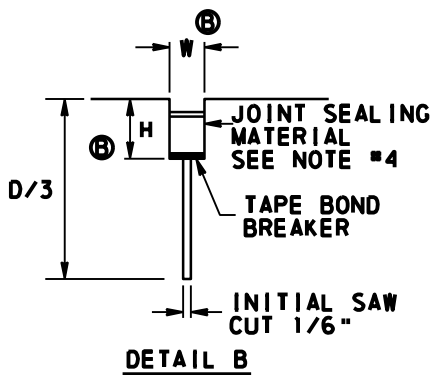
NEW BRIGHTON BOROUGH

CONCRETE SIDEWALK
REPLACEMENT (TYP)

WIDMER ENGINEERING, INC.



JOINT SPACING	W	H
>20'	3/4"	1"
<20'	3/8"	3/4"
PRECAST	1/2"	1"



NOTES

1. WHEN ANY PAVEMENT PATCH REPLACES AN EXISTING EXPANSION JOINT AND THE EXISTING EXPANSION JOINT IN AN ADJACENT LANE REMAINS IN PLACE, INSTALL REMAINING EXPANSION JOINT. PLACE AN APPROVED TUBE HAVING A MINIMUM 1" CLEARANCE POCKET OVER THE LUBRICATED END OF ALL DOWEL BARS IN THE NEW JOINT.
2. USE MINIMUM 1.25" DIA BY 18" LONG DOWEL BARS FOR PAVEMENT DEPTHS 10" OR LESS, AND 1.5" DIA BY 18" LONG DOWEL BARS FOR PAVEMENT DEPTHS GREATER THAN 10". APPROVED ALTERNATE DOWEL BARS HAVING EQUIVALENT PROPERTIES TO CONVENTIONAL ROUND DOWEL BARS MAY BE USED. COATED DOWEL BARS ARE TO BE EITHER GRADE 40 OR GRADE 60.
3. PLACE DOWEL BARS PARALLEL TO THE CENTERLINE AND SURFACE OF THE SLAB. THE VERTICAL OR HORIZONTAL SKEW FROM ONE END OF THE DOWEL BAR TO THE OTHER IS NOT TO EXCEED 1/4".
4. MAKE THE TOP OF THE JOINT SEALING MATERIAL 1/8" TO 1/4" BELOW THE SURFACE OF THE PAVEMENT. JOINT BACKER ROD SHOULD BE 25% TO 35% LARGER THAN THE JOINT WIDTH OPENING AT THE TIME OF SEALANT INSTALLATION.
5. INITIAL SAW CUT IN NOT REQUIRED WHEN EXPANSION JOINT MATERIAL IS USED.
6. SAW AND SEAL DETAILS IN ACCORDANCE WITH DETAIL B OR DETAIL C.
7. VARIANCE IN DIMENSIONS ARE ALLOWED FOR BOTH STANDARD WIDTH PAVEMENT AND OTHER WIDTH PAVEMENT AS LONG AS THE DISTANCE FROM THE EDGE OF PAVEMENT TO THE FIRST DOWEL IS NO LESS THAN 6" AND NO MORE THAN 12", AND THAT THE SPACING OF ALL DOWELS ARE 12" ON CENTER.
8. IF SUBBASE IS REMOVED FOR PATCHING GREATER THAN 20' IN LENGTH AND A MINIMUM OF 8' IN WIDTH, INSTALL CLASS 4, TYPE A GEOTEXTILE AS SPECIFIED IN PENNDOT PUB 408, SECTION 210.
9. SAWCUT JOINT WIDTH AS REQUIRED TO PROVIDE A UNIFORM WIDTH EQUAL TO THE WIDEST WIDTH ALONG THE JOINT AFTER PRECAST SLAB PLACEMENT AND LEVELING.
10. SAWCUT JOINTS TO THE REQUIRED DEPTH TO REMOVE CEMENTITIOUS MATERIAL FOR JOINT SEALANT CONSTRUCTION.
11. CLEAN AND SEAL JOINTS AS SPECIFIED IN PENNDOT PUB. 408, SECTION 501.31(n).

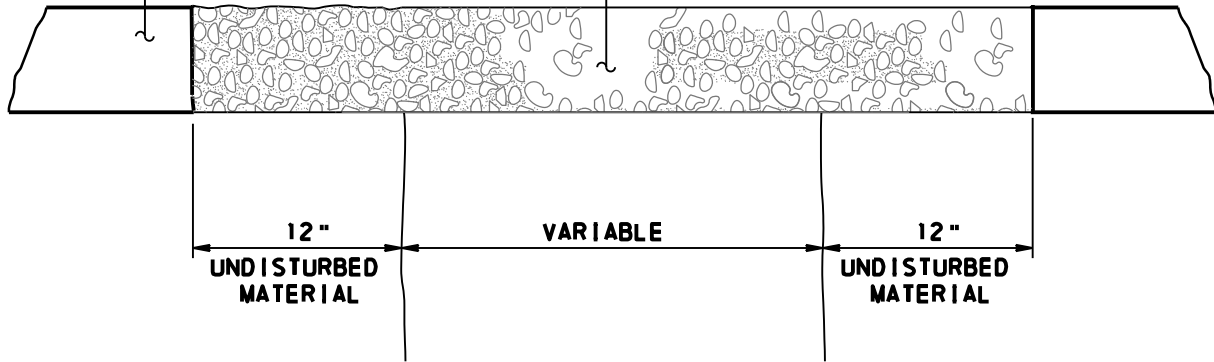
NEW BRIGHTON BOROUGH

CONCRETE PAVEMENT
PATCHING

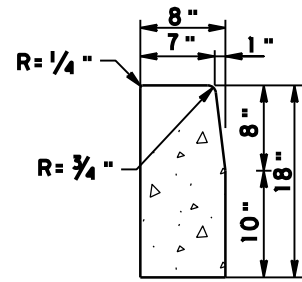
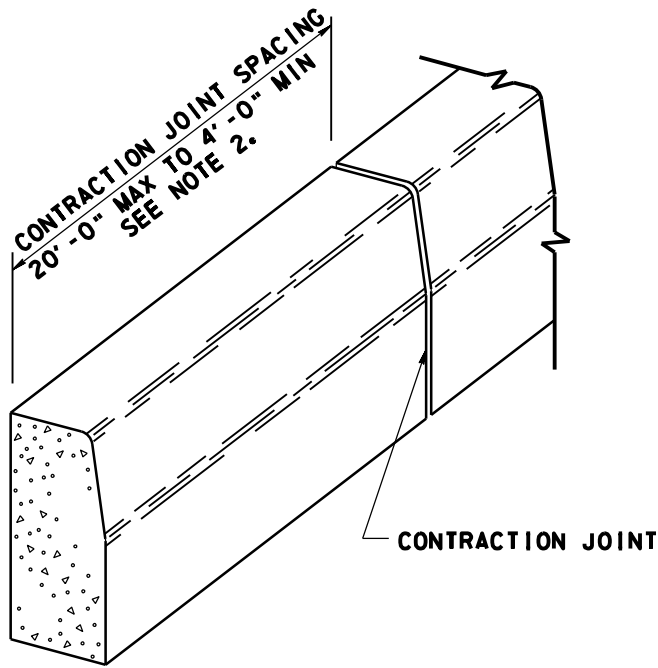
WIDMER ENGINEERING, INC.

TYPE 'A' NO. 57 COARSE AGGREGATE
PENNDOT SEC. 703.2 (NO SLAG)
OR LIMESTONE CHIPS TO MATCH EXIST.
CHOKE AND ROLL, 4" MIN DEPTH

EXISTING
SLAG DRIVEWAY

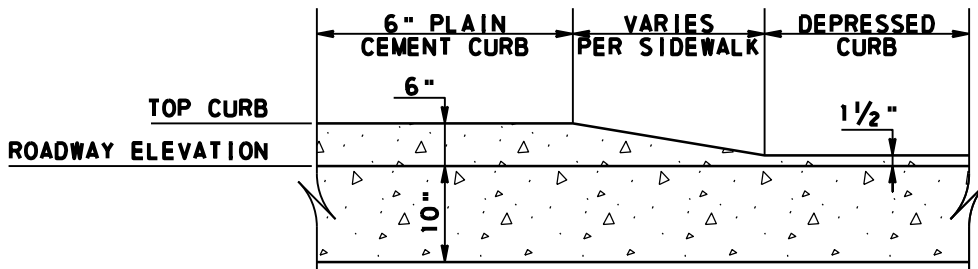


NEW BRIGHTON BOROUGH
SLAG DRIVEWAY RESTORATION
WIDMER ENGINEERING, INC.



**PLAIN CEMENT 8" CURB
TYPICAL CROSS SECTION
NOT TO SCALE**

PLAIN CEMENT CONCRETE CURB

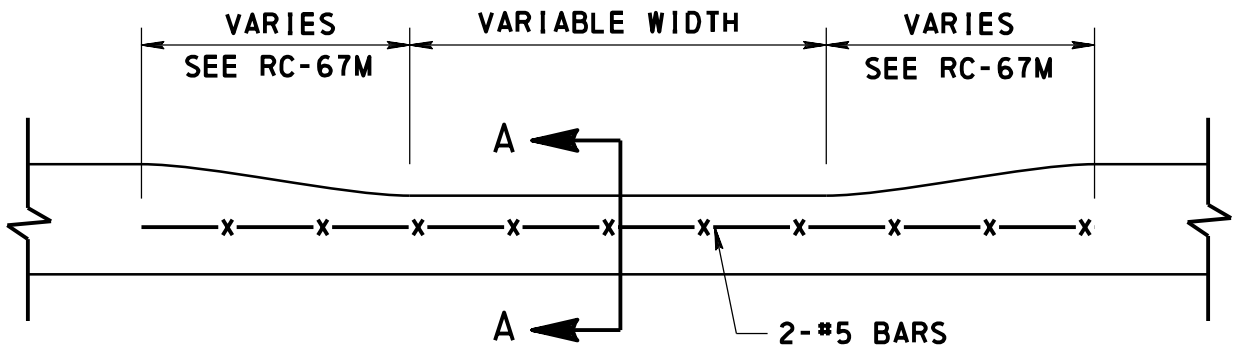


ELEVATION VIEW
NOT TO SCALE

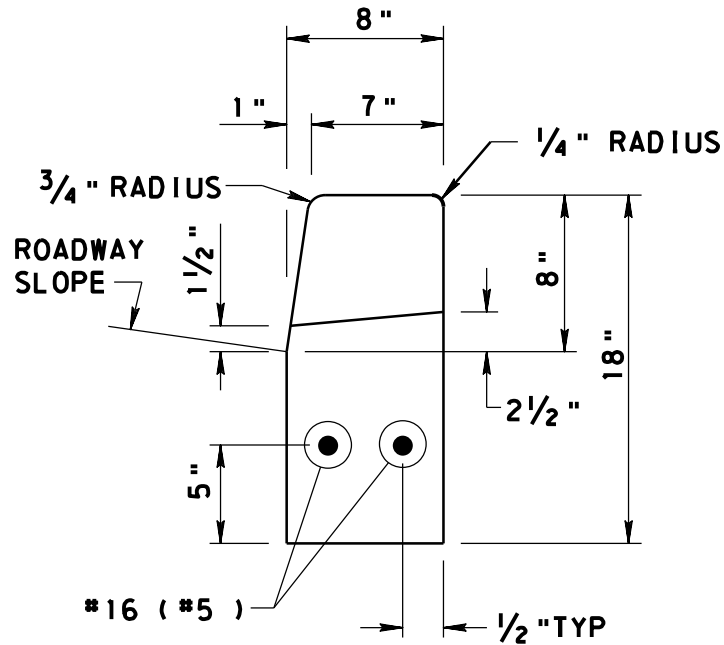
NEW BRIGHTON BOROUGH

CONCRETE CURB
DETAILS

WIDMER ENGINEERING, INC.



ELEVATION VIEW

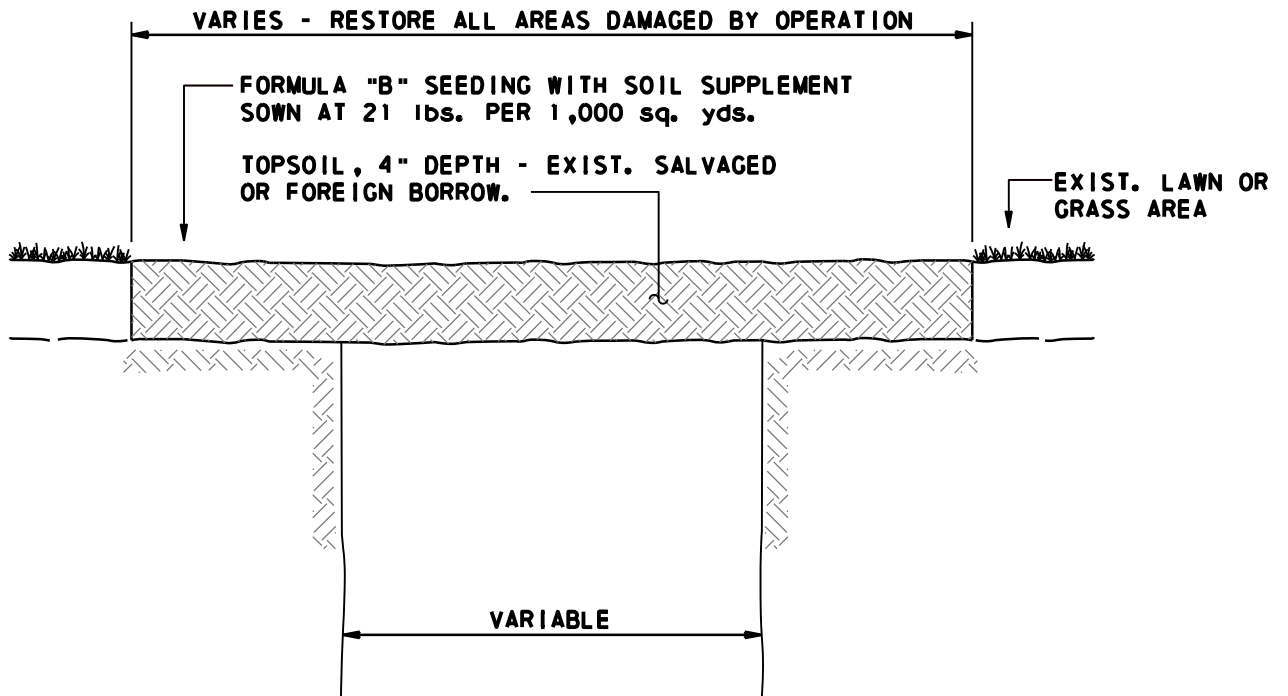


SECTION A-A

NEW BRIGHTON BOROUGH

DEPRESSED CURB
FOR DRIVEWAYS

WIDMER ENGINEERING, INC.



APPLY STRAW OR HAY MULCH AT
1200 lbs. PER 1000 sq. yds. MINIMUM.

NEW BRIGHTON BOROUGH

LAWN AND GRASS
AREA RESTORATION

WIDMER ENGINEERING, INC.